

**March 5, 2007, 5:00 PM - 5:30 PM
TBPOC Conference Call**

Topic	Presenter	Time	Desired Outcome
1. February 2007 Monthly Progress Report – Budget and Forecast Change*	A. Fremier, BATA	5 min	Approval
2. West Approach – Demolition Change Order*	T. Anziano, CT	5 min	Approval
3. Labor Day Weekend Outreach Action Plan *	T. Anziano, CT / E. Millionis, Circlepoint	5 min	Approval
4. TBSRP Fact Sheet *	A. Fremier, BATA	5 min	Approval
5. Update on Cameras Linked to Website	T. Anziano, CT / E. Millionis, Circlepoint	5 min	Information

*Draft Attachments

**Item 1: February Monthly Progress Report –
Budget and Forecast Change**

TO: Toll Bridge Program Oversight Committee (TBPOC) DATE: March 2, 2007

FR: Andrew Fremier, Deputy Executive Director, Bay Area Toll Authority

RE: Agenda No. - 1

Item- February 2007 Monthly Report and Forecast Update Approval

Recommendation:

In April 2007, BATA plans on issuing additional bonds to fund the seismic retrofit program and has requirements to disclose the forecast adjustments.

BATA is requesting that the TBPOC approve cost and schedule forecast adjustments for the South/South Detour (SSD) and Yerba Buena Island Transitions Structures (YBITS) contracts in the February 2007 Monthly Report. The adjustments would be consistent with TBPOC approvals made on February 15, 2007 for the SSD and YBITS contacts.

Cost:

Revised cost forecasts for the SSD and YBITS contracts would result in a net \$180 million cost adjustment for the East Span Project and reduce the forecast program contingency to \$800 million. There is no change to the overall AB144/Approved Program budget.

Discussion:

On February 15, 2007, the TBPOC approved a number of changes to South/South Detour (SSD) and Yerba Buena Island Transition Structures (YBITS) Contracts that have adjusted the cost and schedule forecasts for the contracts. Based on current TBPOC guidance, these cost and schedule forecasts would not be updated until the 1st Quarter 2007 Report in May 2007, which would be too late for BATA's disclosure requirements.

BATA is requesting that the TBPOC approve forecast adjustments in the February 2007 Monthly Report. The report would be issued to the BATA Oversight Committee on March 7, 2007 with cost and schedule forecast adjustments consistent with prior TBPOC approvals for the SSD and YBITS contacts.

Following is the monthly report write-up that discusses and summarizes the actions of the TBPOC on the SSD and YBITS contracts. The main write-up is on page 18 of the draft February Monthly Progress Report.

Contract Status: The South/South Detour (SSD) contract was awarded in early 2004 to construct a temporary detour structure providing for, at that time, a new bridge opening in 2006. Due to the re-advertisement of the SAS superstructure

contract in 2005, bridge opening was rescheduled to 2013, which necessitated a temporary suspension of the contract and design changes.

In 2006, the TBPOC approved a plan to pace work on the project, to have Caltrans assume design responsibility over the east and west tie-ins, and to make changes to the detour structures to allow it to stand in place alone for a longer duration than originally intended. The SSD contract is now forecasted to be completed in 2010 in time for the revised opening date of the new bridge.

In addition to the revised contract completion date, the TBPOC approved on February 15, 2007 to advance foundation and retrofit work from the Yerba Buena Island Transition Structures (YBITS) contract to the South/South Detour contract. Advancing the work will reduce overall project schedule risk by taking work off the critical path for the East Span project while making more effective use of the extended SSD contract duration.

Advancing the transition structure work, completing the tie-in work under Caltrans design, and pacing of the remaining SSD work will result in a \$220 million increase on the SSD contract and a \$40 million decrease on the YBITS contract for a net \$180 million increase in the project costs. The increase will be covered by the existing program contingency and will not increase the AB144 program budget.

Prior to the suspension of the SSD contract, foundations for the temporary detour were nearly completed. Work has restarted on the contract, including fabrication of the temporary viaduct Korea and the start on the YBITS advanced work. The contractor has already made significant progress on the foundation and column at pier W3 of YBITS and has started work on retrofitting of the upper deck approach to the Yerba Buena Island Tunnel. The upper deck approach retrofit will require a three-day closure of the Bay Bridge to roll in a replacement upper roadway. Currently, the closure is scheduled for as early as Labor Day 2007.

The revised contract completion date for the SSD contract is June 2010. The overall YBITS contract completion date remains November 2014. Both completion dates would be accelerated if early opening dates in the opportunity schedule were met.

The revised cost forecast adjustments would result in a reduction of the program contingency from \$989 million to \$800 million. A table from the monthly report showing the results of the change to the program is attached.

It should also be noted that the program contingency is anticipated to change as risks are identified and contracts are completed. Completion of the Skyway and the E2/TI SAS foundations in late 2007 and early 2008, respectively, should yield savings back to the program contingency.

Executive Summary

Toll Bridge Seismic Retrofit Program—Cost (\$Millions)

Project	Work Status	AB 144 / SB 66 Budget (07/2005)	Approved Changes	Current Approved Budget (12/2006)	Cost To Date (12/2006)	Cost Forecast*	At- Completion Variance	Cost Status
A	b	c	d	e = c + d	f	g	h = g - e	i
SFOBB East Span Replacement Project								
Capital Outlay Support		959.4	-	959.4	466.7	977.1	17.7	●
Capital Outlay Construction								
Skyway	Construction	1,293.0	-	1,293.0	1,114.4	1,293.0	-	●
SAS E2/T1 Foundations	Construction	313.5	-	313.5	194.3	313.5	-	●
SAS Superstructure	Construction	1,753.7	-	1,753.7	215.6	1,767.4	13.7	●
YBI South/South Detour	Design/Const	131.9	-	131.9	37.2	334.4	202.5	●
YBI Transition Structures	Design	299.3	-	299.3	-	276.1	(23.2)	●
Oakland Touchdown (OTD)		283.8	-	283.8	-	302.5	18.7	
* OTD Submarine Cable	Construction	-	-	-	-	9.6	-	●
* OTD No. 1 (Westbound)	Advertised	-	-	-	-	226.5	-	●
* OTD No. 2 (Eastbound)	Design	-	-	-	-	62.0	-	●
* OTD Electrical Systems	Design	-	-	-	-	4.4	-	●
Existing Bridge Demolition	Design	239.2	-	239.2	-	222.0	(17.2)	●
Stormwater Treatment Measures	Construction	15.0	-	15.0	6.4	15.0	-	●
East Span Completed Projects		90.3	-	90.3	88.6	90.3	-	
Right-of-Way and Environmental Mitigation		72.4	-	72.4	38.8	72.4	-	●
Other Budgeted Capital		35.1	-	35.1	0.6	11.0	(24.1)	
Total SFOBB East Span Replacement Project		5,486.6	-	5,486.6	2,162.6	5,674.7	188.1	
SFOBB West Approach Replacement								
Capital Outlay Support	Construction	120.0	-	120.0	86.9	120.0	-	●
Capital Outlay Construction		309.0	-	309.0	224.7	309.0	-	
Total SFOBB West Approach Replacement		429.0	-	429.0	311.6	429.0	-	
Richmond-San Rafael Bridge Retrofit								
Capital Outlay Support	Complete	134.0	(7.0)	127.0	125.7	127.0	-	●
Capital Outlay Construction & Right-of-Way		780.0	(82.0)	698.0	665.6	698.0	-	
Total Richmond-San Rafael Bridge Retrofit		914.0	(89.0)	825.0	791.3	825.0	-	
Program Completed Projects								
Capital Outlay Support	Complete	219.8	-	219.8	219.4	219.8	-	
Capital Outlay Construction		705.6	-	705.6	698.1	705.6	-	
Total Program Completed Projects		925.4	-	925.4	917.5	925.4	-	
Miscellaneous Program Costs		30.0	-	30.0	24.7	30.0	-	
Program Contingency		900.0	89.0	989.0	-	800.9	(188.1)	
Total Toll Bridge Seismic Retrofit Program		8,685.0	-	8,685.0	4,207.7	8,685.0	-	

● Within Approved Current Schedule and Budget

● Potential Cost and Schedule Impacts: Possible future need for Program Contingency Allocation

● Known Cost and Schedule Impacts: Request for Program Contingency Allocation forthcoming

Note: Details may not sum to totals due to rounding effects.

* Cost forecasts are as of August 31, 2006. Forecasts for the Monthly Reports are generally updated on a quarterly basis in conjunction with Risk Analysis assessments for the TBSRP Projects and the TBSRP Quarterly Reports.

Toll Bridge Seismic Retrofit Program—Schedule

Project	AB 144 / SB 66 Project Complete Baseline (07/2005)	Approved Changes (Months)	Project Complete Current Approved Schedule (12/2006)	Project Complete Schedule Forecast (12/2006)	Schedule Variance (Months)	Schedule Status	Remarks
A	b	c	d= b + c	e	f = e – d	g	h
SFOBB East Span Replacement Project Skyway	Apr 07	8	Dec 07	Dec 07	-	●	See page Error! Bookmark not defined..
SAS E2/T1 Foundations	Jun 08	(3)	Mar 08	Mar 08	-	●	See Note.
SAS Superstructure	Mar 12	12	Mar 13	Mar 13	-	●	
YBI South/South Detour	Jul 07	36	Jun 10	Jun 10	-	●	
YBI Transition Structures	Nov 13	12	Nov 14	Nov 14	-	●	In March 2006, the TBPOC approved the split of the YBI contract into three contracts. Schedules and estimates for the split contracts are being developed
Oakland Touchdown (OTD)	Nov 13	12	Nov 14	Nov 14	-	●	
• OTD Submarine Cable	n/a		Jan 08	Jan 08	-	●	Contract was awarded on January 11, 2007. See pages 9 and Error! Bookmark not defined..
• OTD Westbound	n/a		Jul 09	Oct 09	3	●	Advertise date postponed to provide additional time for utility coordination and contract formation.
• OTD Eastbound	n/a		Nov 14	Nov 14	-	●	See Note.
Existing Bridge Demolition	Sep 14	12	Sep 15	Sep 15	-	●	See Note.
Stormwater Treatment Measures	Mar 08	-	Mar 08	Jun 07	(9)	●	Forecast based on actual award date and duration in contractor's A+B bid.
Open to Traffic Date: Westbound	Sep 11	12	Sep 12	Sep 12	-	●	See Note.
Open to Traffic Date: Eastbound	Sep 12	12	Sep 13	Sep 13	-	●	See Note.
SFOBB West Approach Replacement	Aug 09	-	Aug 09	Aug 09	-	●	
Richmond-San Rafael Bridge							
• Seismic Retrofit	Aug 05	-	Aug 05	Oct 05	2	●	Seismic retrofit completed July 29, 2005. Formal acceptance of contract October 28, 2005. \$89 million has been transferred to Program Contingency. See page Error! Bookmark not defined..
• Public Access Project	n/a	-	May 07	May 07	-	●	

Note: Schedules for selected projects and the Open to Traffic dates were extended by 12 months from the AB144/SB66 baseline schedule due to Addenda #5 and #7 on the SAS Superstructure contract.

Highlights of Project/Program Activities and TBPOC Actions for February 2007

Toll Bridge Seismic Retrofit Program

SFOBB East Span Seismic Replacement Project

- ◆ On the Yerba Buena Island (YBI) South/South Detour and Transition Structures contracts, the TBPOC approved on February 15, 2007 to advance foundation and retrofit work from the Transition Structure contract to the South/South Detour contract. Advancing the work will reduce overall project schedule risk by taking work off the critical path for the East Span project and will result in a net \$180 million increase in the project costs that will be covered by the existing program contingency and will not increase the AB144 program budget. Advancement of retrofit work near the Yerba Buena Island Tunnel will require a three-day closure of the Bay Bridge to replace the upper roadway from the east span to the tunnel. Currently, the closure is scheduled for as early as Labor Day 2007 (see page 18).
- ◆ On the Self-Anchored Suspension Span (SAS) Superstructure Contract, the American Bridge/Flour Joint Venture (ABF) submitted a baseline schedule to Caltrans for review on February 2, 2007. Zhenhua Port Machinery Company (ZPMC) of Shanghai, China is currently setting up their facilities to fabricate the steel tower and deck sections. ZPMC is preparing initial test mock-ups of the sections and plans to begin production fabrication later in 2007. ABF completed the design of the crane barge to be used to lift the heavy tower and deck sections. Barge fabrication has started in Oregon (see page 15).
- ◆ On the SAS Marine Foundations Contract, all 13 rock sockets that tie the SAS tower foundation (T1) to bedrock have been installed. The T1 footing box has been fabricated and is being shipped to the Bay Area via the Panama Canal from Texas (see page 13).
- ◆ On the Submerged Electrical Cable Relocation Contract, Caltrans approved the contract on January 22, 2007. The contractor has submitted a purchase order to procure the two cables to be installed from Oakland to Treasure Island. The cables will be fabricated in Italy and shipped to the US for installation later in 2007 (see page 21).
- ◆ On the Oakland Touchdown #1 (OTD1) contract, Caltrans advertised the contract on February 26, 2007. Bid opening is scheduled for June 5, 2007 (see page 22).

SFOBB West Approach Seismic Retrofit Project

- ◆ In late March to early April 2006, Caltrans plans to shift the eastbound Interstate 80 (I-80) traffic alignment under the new westbound structure from the Fifth Street to Second Street in San Francisco to allow for the demolition and reconstruction of the eastbound structure along Bryant Street. No closures of the Bay Bridge will be necessary for the traffic shift (see page 27).

Regional Measure 1 Program

I-880/SR-92 Interchange Project

- ◆ Caltrans advertised this contract on January 8, 2007. Bid opening is scheduled for May 5, 2007 pending right-of-way certification clearance in April 2007. Expected duration of this particular project is for four (4) years. (See Page 47).

**Item 2: West Approach Demolition Change
Order**

CONTRACT CHANGE ORDER MEMORANDUM

DATE: 1/22/2007 Page 1 of 2

TO: Dennis Turchon / Deanna Vilcheck		FILE: E.A. 04 - 0435V4		DRAFT
FROM: Deanna Vilcheck		CO-RTE-PM SF-80-4.9/5.9		
		FED. NO.		
CCO#: 208	SUPPLEMENT#: 0	Category Code: CHTC	CONTINGENCY BALANCE (incl. this change) \$0.00	
COST: \$1,200,000.00	INCREASE <input checked="" type="checkbox"/>	DECREASE <input type="checkbox"/>	HEADQUARTERS APPROVAL REQUIRED? <input checked="" type="checkbox"/> YES <input type="checkbox"/> NO	
SUPPLEMENTAL FUNDS PROVIDED: \$0.00		IS THIS REQUEST IN ACCORDANCE WITH ENVIRONMENTAL DOCUMENTS? <input checked="" type="checkbox"/> YES <input type="checkbox"/> NO		
CCO DESCRIPTION: Stage 5 Mainline Demo Resequencing		PROJECT DESCRIPTION: SEISMIC RETROFIT		
Original Contract Time: 1824 Day(s)	Time Adj. This Change: 0 Day(s)	Previously Approved CCO Time Adjustments: 52 Day(s)	Percentage Time Adjusted: (including this change) 3 %	Total # of Unreconciled Deferred Time CCO(s): (including this change) 0

THIS CHANGE ORDER PROVIDES FOR:

Resequencing the Contractor's planned Stage 5 bridge removal work in order to mitigate past and future Department delays and address concerns of the abutting neighborhood communities.

Over the time period from September 2006 to February 2007, the contract incurred approximately 2 months of Department delay to the controlling activities on Frames 1 and 2 of the mainline lower deck, the 5th Street On-Ramp structure and the Interim Eastbound Detour structure. These delays were incurred due to both changes made to the pile construction sequencing implemented under Change Order No. 71 and the addition of a pile, column and bent to the planned Interim Eastbound Detour structure implemented under Change Order No. 176.

Due to these delays, the planned start of the Stage 5 demolition work has been pushed to late March 2007. This bridge removal work consists of demolishing the remaining portion of the upper deck Westbound I-80 structure and the entire lower deck Eastbound I-80 structure at the western approach to the San Francisco Oakland Bay Bridge located in downtown San Francisco. The work takes place over several major city streets and abuts directly against numerous city residences and businesses. The contractor's submitted demolishing plan calls for up to 8 weekends of street closures to complete the demolition work.

The contract allows for weekend closures of the city streets for demolition work but blocks out any weekends associated events at the nearby AT&T Park. This weekend block out includes any home games played by the San Francisco Giants baseball team.

Due to Department delays, the bridge removal work will now take place during the Giants baseball season thus blocking out half of the weekends to street closures. This would draw out the planned 8 weekends of street closures over a 16 week period causing additional delay to the project. In order to avoid this delay, this change order shall modify the contractor's demolition schedule so that only two weekend closures are required. This will be accomplished by mobilizing additional equipment, and working multiply shifts (24 hours per day) during the closure periods.

The resequencing should also act to mitigate past Department delays. A time impact analysis shows a potential 8 week time savings to the project resulting from this change order when considering both the reduced as-planned demolition schedule and the avoidance of further delays due to Giants games.

The cost of a one day contract time extension to the project is estimated at approximately \$40,000. This includes over \$15,000 per day for time related overhead, approximately \$15,000 per day for idle equipment including temporary railing, falsework and dedicated project equipment and \$10,000 per day for idle equipment and labor associated with the demolition subcontractor. Mitigating 8-weeks of project delay at a cost of \$40,000 per day would result in a savings of \$2,240,000.

The reduced demolition period has been presented to the City and the neighboring community and has received overwhelming support from both.

The costs of the change order include mobilizing additional equipment, premium time labor and inefficiencies due to the compressed demolition schedule. This work shall be paid as an adjustment of compensation at an agreed lump sum not to exceed \$1,200,000.00 which shall be financed from the contract's contingency fund.

Adjustment of contract time is deferred as the change is expected to mitigate Department delays.

CONTRACT CHANGE ORDER MEMORANDUM

EA: 0435V4 CCO: 208 - 0

DATE: 1/22/2007

Page 2 of 2

Maintenance concurrence is not required as the work doesn't affect any permanent roadway.

DRAFT

CONCURRED BY:			ESTIMATE OF COST		
Construction Engineer:	D. Vilcheck	Date	THIS REQUEST		TOTAL TO DATE
Bridge Engineer:	W. Basha	Date	ITEMS	\$0.00	\$0.00
Project Engineer:	H. Wong	Date	FORCE ACCOUNT	\$0.00	\$0.00
Project Manager:	A. Melkonians	Date	AGREED PRICE	\$0.00	\$0.00
FHWA Rep.:		Date	ADJUSTMENT	\$1,200,000.00	\$1,200,000.00
Environmental:		Date	TOTAL	\$1,200,000.00	\$1,200,000.00
Other (specify):	Traffic Ops	Date	FEDERAL PARTICIPATION		
Other (specify):	City of San Francisco	Date	<input type="checkbox"/> PARTICIPATING <input type="checkbox"/> PARTICIPATING IN PART <input checked="" type="checkbox"/> NONE <input type="checkbox"/> NON-PARTICIPATING (MAINTENANCE) <input type="checkbox"/> NON-PARTICIPATING		
District Prior Approval By:		Date	FEDERAL SEGREGATION (if more than one Funding Source or P.I.P. type)		
HQ (Issue Approve) By:		Date	<input type="checkbox"/> CCO FUNDED PER CONTRACT <input type="checkbox"/> CCO FUNDED AS FOLLOWS		
Resident Engineer's Signature:		Date	FEDERAL FUNDING SOURCE PERCENT		

CONTRACT CHANGE ORDER

Change Requested by: Engineer

CCO 208 Suppl. No. 0 Contract No. 04 - 0435V4 Road SF-80-4.9/5.9

FED. AID LOC.:

To: TUTOR-SALIBA CORP

You are directed to make the following changes from the plans and specifications or do the following described work not included in the plans and specifications for this contract. **NOTE: This change order is not effective until approved by the Engineer.**

Description of work to be done, estimate of quantities and prices to be paid. (Segregate between additional work at contract price, agreed price and force account.) Unless otherwise stated, rates for rental of equipment cover only such time as equipment is actually used and no allowance will be made for idle time. This last percentage shown is the net accumulated increase or decrease from the original quantity in the Engineer's Estimate.

The Stage 5 bridge removal work (Bridge Removal, Location M) concerning both the upper and lower deck structures (Bridge No. 34-0126R/L) shall be sequenced and completed under the following schedule:

- 1) The first weekend after eastbound traffic is placed onto the Interim Eastbound Detour (ST6D line), where the full closure of 2nd Street is permitted under Section 10-1.29 "Maintaining Traffic" of the Contract Special Provisions, the Contractor shall demolish from Bent 30 through 37 of the upper deck and from Bent 30 through 35 of the lower deck. This work shall be performed and completed during the time period from 10 PM Friday evening to 5 AM Monday morning. During this time period, the Contractor may implement a full closure of 2nd Street and close all access to the northern entrance to the parking garage of the Clock Tower Building (461 2nd Street).
- 2) The second weekend after eastbound traffic is placed onto the Interim Eastbound Detour, where the full closure of 3rd Street is permitted under Section 10-1.29 "Maintaining Traffic" of the Contract Special Provisions, the Contractor shall demolish from Bent 6 through 22 of the lower deck. This work shall be performed and completed during the time period from 10 PM Friday evening to 5 AM Monday morning. During this time period, the Contractor may implement a full closure of 3rd Street and a full closure of Stillman Street between 3rd Street and 4th Street.
- 3) The Contractor shall complete the demolition of the remaining Stage 5 bridge removal work (Bridge Removal, Location M) within 2 weeks after the first weekend closure.
- 4) The Contractor shall complete the concrete processing from the Stage 5 bridge removal work within 4 weeks after the second weekend closure.

DRAFT

Adjustment of Compensation at Lump Sum:

For all costs associated with the sequencing of the Stage 5 bridge removal work (Bridge removal, Location M) as specified under this change order, the Contractor shall be compensated an agreed lump sum not to exceed \$1,200,000.00. This sum constitutes full and final compensation, including all markups, for all costs resulting from this change.

Compensation provided under this change order includes all costs associated with the sequencing of the Stage 5 bridge removal work as specified above. These costs include, but are not limited too, mobilizing additional equipment, labor premium time and inefficiencies, traffic control and all support costs necessary for this work. No additional compensation shall be paid for this change.

Cost of Adjustment of Compensation at Agreed Lump Sum (NOT TO EXCEED)\$1,200,000.00

The sound control requirements set forth in Section 7-1.011 of the Standard Specifications and those set forth in Section 5-1.30 of the Special Provisions shall be waived from 10:00 PM on Friday through 5:00 AM on the subsequent Monday as they pertain to the 2 weekend closures specified under this change order. However, due diligence shall be used to reduce noise levels as much as possible.

Failure to open 2nd Street, 3rd Street, Stillman Street or the access to the northern entrance to the parking garage of the Clock Tower Building by 5 AM Monday morning shall result in the Department deducting \$8,500.00 per 10-minute interval for the late opening in accordance with "Late Reopening of Closures" of Section 10-1.30 of the Special Provisions.

Except for the 2 weekends specified under this change order, no additional full weekend closures of 2nd Street, 3rd Street, Stillman Street or the access to the northern entrance to the parking garage of the Clock Tower Building will be allowed for

CCO 208

Suppl. No. 0

Contract No. 04 - 0435V4

Road SF-80-4.9/5.9

FED. AID LOC.:

the Stage 5 bridge removal work.

Should Contractor-Controlled Insurance Program costs apply, these costs will be determined separately and compensated by the Department.

DRAFT

Estimated Cost: Increase ☒ Decrease ☐

\$1,200,000.00

By reason of this order the time of completion will be adjusted as follows: Deferred

Submitted by

Signature	Resident Engineer	Date
	Deanna Vilcheck	

Approval Recommended by

Signature	Construction Engineer	Date
	Dennis Turchon	

Engineer Approval by

Signature	Construction Manager	Date
	Dennis Turchon	

We the undersigned contractor, have given careful consideration to the change proposed and agree, if this proposal is approved, that we will provide all equipment, furnish the materials, except as may otherwise be noted above, and perform all services necessary for the work above specified, and will accept as full payment therefor the prices shown above.

NOTE: If you, the contractor, do not sign acceptance of this order, your attention is directed to the requirements of the specifications as to proceeding with the ordered work and filing a written protest within the time therein specified.

Contractor Acceptance by

Signature	(Print name and title)	Date
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Item 3: Labor Day Weekend Outreach Action Plan

DRAFT



STAKEHOLDER AND MEDIA OUTREACH ACTION PLAN

YERBA BUENA ISLAND VIADUCT REPLACEMENT

FOR NEW EAST SPAN

Labor Day Weekend, August 31 - September 3, 2007

OVERVIEW

This report outlines the proposed outreach elements that will be implemented to inform stakeholder entities and the public about upcoming work on the Yerba Buena Island Viaduct Replacement, as part of the Bay Bridge Seismic Safety Projects. The outreach effort for this operation will build upon the successes and lessons learned from the previous operations on the West Approach requiring full lower deck closures of the Bay Bridge. Because the upcoming closures involve both upper and lower decks (eastbound and westbound directions), outreach efforts will be broadened in scope and geography for the upcoming closure. Going beyond past efforts, Caltrans will expand coordination with East Bay cities and counties, conduct advance planning with event venues, increase the distribution of information to statewide audiences, and target Labor Day Weekend travelers into and out of the Bay Area.



Digital rendering of YBI Viaduct replacement structure

SECTION ONE ELECTED OFFICIALS OUTREACH

Caltrans will inform elected officials directly, regarding the upcoming Bay Bridge Closures.

1.1 Outreach Meetings

Caltrans will provide multimedia presentations to project stakeholders on the upcoming work. Elected officials from the Bay Area will be invited to two presentations given by the Bay Bridge Spokesperson, Bart Ney, and East Span Construction staff in April, 2007 (up to 21 weeks in advance of the closures). Invitations will be extended to the offices of Senators Boxer and Feinstein, all members of the Bay Area Congressional delegation, all Bay Area state Senators, all Assembly members from the Bay Area, as well as Supervisors in all nine Bay Area counties, and mayors in key Bay Area cities. Caltrans staff will also contact the affected Transportation Authorities, Mayor's Offices, and the Metropolitan Transportation Commission/Bay Area Toll Authority Commission. Graphics and informational fact sheets will be distributed.

1.2 E-Alert

Electronic alerts will be sent to all elected officials and staff contacts, providing information on the upcoming demolition and link to a Fact Sheet which could be viewed electronically, shared, or printed in hardcopy. The first notification will serve as advance notice, and a second E-Alert will serve as a reminder a few days prior to the beginning of the operation.

SECTION TWO MEDIA OUTREACH

Caltrans will inform the media prior to, during and after all major elements of the work.

2.1 Media Outreach Sessions

Media in the San Francisco Bay Area and in surrounding media markets will be invited to an educational outreach session during April 2007, up to 20 weeks in advance of the upcoming work. A separate media outreach will be held in Sacramento. Depending on the level of interest, a third media outreach session may be scheduled in Southern California. Graphics, video, and informational Fact Sheets will be distributed. These sessions are intended to raise media awareness, inform media of upcoming work, provide current contact information, foster collaborative working relationships and solicit feedback on how to improve our outreach.

2.2 Press Releases

Caltrans will distribute a general press release prior to the weekend closures. A media press release will be issued at the completion of the operation to keep media updated on the completion and re-opening of the Bay Bridge.

2.3 Public Information Officer Live Update

The Caltrans Bay Bridge Public Information Officer (PIO) will be on site throughout the weekend operation. A media hold location will be made available throughout the weekend at an appropriate location that provides an exclusive view of the operation. Live updates to the media will be facilitated at this location. Caltrans will develop talking points ahead of time and construction staff will provide real-time construction updates to the PIO for sharing with media.

SECTION THREE PUBLIC OUTREACH

Caltrans will inform the public through a broad outreach campaign designed to inform as many potential weekend users of the Bay Bridge as possible. The targeted user groups will include Bay Area motorists, regional commuters, goods movement industries, out-of-town holiday travelers, the general public and immediate neighborhood residents. Notices will be provided months in advance in some cases.

3.1 **Public Service Announcements**

Paid public service announcements will run in television, print, radio and movie theater media to share information with the general public three to four weeks in advance of the beginning of work. Markets throughout the state will be targeted. Detailed graphics will be included in the messaging to help show the public the work that will be performed. Messaging will focus on keeping traffic away from the bridge approaches and encourage motorists to seek alternative transit and driving options.

3.2 **Website**

All outreach materials will direct stakeholders to the BayBridgeInfo.org website for daily information and updates about the work, and the associated ramp and deck closures. This includes graphical and text information on the work and the schedule; information on the transit alternatives available, including links to each transit operator and to 511; links to radio and television announcements, and other informational materials. The website includes a comment form for users to send questions or feedback 24 hours/day as well as contact phone and address information for the Public Information Office and telephone hotline.

3.3 **Mailers and Flyers**

Caltrans will develop informational materials, including a Fact Sheet, for distribution electronically, through the mail and at public locations near the upcoming work. The Fact Sheet includes dates and times of work and the associated deck closures, the rationale for conducting this operation over Labor Day Weekend, transit and driving alternatives, as well as background information on the Bay Bridge Seismic Safety Projects.

Distribution

Fact Sheets will be distributed in hardcopy (including mailers) and in electronic formats to:

- Local/corridor businesses
- Neighborhood newsletters and other publications
- Treasure Island Development Authority and Mayor's Office staff
- Residential neighbors, including all Treasure Island/YBI residents
- Taxis and shuttle services, airports, hotels, car rental agencies, visitor's bureaus, the State Tourism Office, Chambers of Commerce and automobile associations
- Hospitals, major employers, funeral homes, farmers markets, carpool centers, parking garages, malls
- Major regional and local entertainment and sports venues for the SF 49ers, the Oakland Athletics, the SF Giants, and the Oakland Raiders. Caltrans will also contact university sports venues, including UC Berkeley, Stanford, and local Cal State campuses, regarding home games over the Labor Day weekend.
- Cities from San Luis Obispo to Sacramento in the target market areas (Bay Area, Central Valley, Southern California, Sacramento)

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- Ferry operators, bus transit and rail operators, transit centers, Bay Area Rapid Transit, the Water Transit Authority, and the San Francisco Metropolitan Transportation Agency
- San Francisco Municipal Railway (MUNI)
- State and local offices of the California tourism agencies and convention bureaus
- Approximately 5,000 organizations and private citizens on the Bay Bridge Public Information Office contacts list

3.4 Banners

Caltrans will post banners at multiple locations to guide the public on where to go for more information on the upcoming work and motorist impacts. The banners will be posted in advance and will point motorists and the public to the BayBridgeInfo.org website, and 511.

3.5 Local Notification

Presentations and notices will be given to Treasure Island residents and any other residential or commercial locations that might be specifically affected by access restrictions, noise, dust, and vibration. The 24-hour telephone hotline also serves to provide nightly construction updates and receive questions and comments.

3.6 Telephone Hotline

Caltrans provides a telephone hotline at the Public Information Office for motorists to receive daily updates on construction-related lane and ramp closures and other construction information, and for local affected residents and businesses to have direct contact with PIO staff. The hotline will be staffed for extended hours during the weeks leading up to and throughout Labor Day Weekend.

3.7 Changeable and Electronic Message Signs (CMS's)

Caltrans will engage a statewide network of electronic and changeable message signs two weeks prior to the closures to alert motorists. Signs will be especially intensive in the Bay Area; Caltrans will work closely with Districts throughout the state to ensure that the message will be highly visible along major thoroughfares.

3.8 Highway Advisory Radio (HAR)

The Bay Bridge Public Information Team will script the message and provide it to the Caltrans operations unit for posting on the HAR frequencies. Caltrans promotes the HAR on the banners posted within range of the HAR frequency.

3.9 E-Alert

Similar to the E-Alert sent to elected officials, an electronic alert will be sent to the general public. Thousands of project contacts will receive the E-Alert well in advance of the closures, providing information on the upcoming demolition and linking to a Fact Sheet which could be viewed electronically, shared, or printed in hardcopy. An additional (reminder) E-Alert will be sent a few days before the closure.

3.10 Out-of-town Traveler Notification

Caltrans will focus additional efforts to target out-of-town travelers visiting the Bay Area during the Labor Day Weekend, who might be impacted by the Bay Bridge closure. Many elements of the outreach plan will be implemented earlier than in past efforts, and extended to additional metropolitan regions in California. Visitor Bureaus, recreational venues, and other traveler services will be included in all possible aspects of the outreach plan. Information will be distributed to hundreds of California cities, the Weather Channel

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and on the California Department of Tourism website. Information kiosks at major airports in the Bay Area throughout the four-day operation will provide information.

3.11 Transit Agency Coordination

Caltrans will coordinate on an ongoing basis with BART, AC Transit, MUNI, Golden Gate Transit, Samtrans, Vallejo Ferry, Alameda/Oakland Ferry, Caltrain, Greyhound and Amtrak to inform transit riders of the upcoming lower deck closures. Each of the agencies will distribute information to riders and staff. In addition, MUNI buses will display placards. Throughout the Labor Day Weekend operation, daily updates will be given to the other bridges (Golden Gate, San Mateo-Hayward, Dumbarton, Richmond-San Rafael) on traffic and operational progress.

3.12 MTC 511 Coordination

Caltrans will continue to collaborate with MTC staff responsible for the 511 Transit Information system on the upcoming work and the changes to transit schedules as a result of the closures. MTC incorporates the revised schedule information on their voice-activated system and the MTC 511 (www.511.org) website. Furthermore, MTC posts a graphic banner announcing the Bay Bridge Construction and Closures on the homepage pointing users to BayBridgeInfo.org for information.

3.13 Department Informational Letter

Caltrans distributes an informational fact sheet electronically to District 4 staff on the upcoming work. The Fact Sheet includes dates and times of work and the associated closures, as well as transit and driving alternatives.

3.14 Coordination with other Caltrans Districts

Caltrans works with other Districts to extend messaging on key highway Changeable Message Signs in those districts, as well as in distributing Fact Sheets to all District staff.

SECTION FOUR CALTRANS INTERNAL COORDINATION

4.1 **Command Center**

Caltrans staff will continue to hold regular meetings to review ongoing public issues relating to the project. During the operation, a Command Center equipped with computers, television monitors, workspaces and meeting space will be established for all key agencies to be able to work on site and coordinate closely together.

4.2 **District 4 Coordination**

Public Affairs Office

The Bay Bridge Public Information staff communicates regularly with the District 4 Public Affairs staff to help ensure that district staff is informed and to identify potential areas for collaboration.

District Director's Office

Presentations on the public outreach strategy and implementation elements will be made to the District Director and Director's Staff in March 2007.

Traffic Operations

Caltrans holds intermittent meetings between key District operations staff on all of the projects along the Bay Bridge Corridor. The Traffic Management Center addresses the anticipated needs of the operation by joining the Command Center, and by assisting on the public outreach effort through the operational elements, such as Changeable Message Signs.

4.3 **Agency and Executive Staff**

CT Headquarters, including the Director and the TBPOC agencies, are given a presentation on the scope and impacts of the work prior to the beginning of work. The TBPOC will review and approve the Outreach Action Plan in March 2007. Caltrans Headquarters (Lane Closure Review Committee) will be briefed in March 2007. Regular communications and updates on the public outreach strategy and implementation will be made to the Public Affairs Office, the Caltrans Director and Director's Staff.

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SECTION FIVE
PROPOSED PRESENTATION CALENDAR

March 2007	District Executive Staff Presentation Caltrans Lane Closure Review Committee Presentation Key Stakeholder Presentations (Including TIDA, CCSF, SF Giants, Oakland A's, UC Berkeley (Cal) Football, Oakland Art & Soul Festival, Golden Gate Bridge, Cities of Hayward, Marin, Larkspur, San Rafael, County Transportation Authorities)
April 2007	Elected Officials Legislative Outreach Meetings Media Outreach Meeting Transit Agency Coordination
May 2007	Establish PIO Office on Treasure Island Telephone Hotline
June - July 2007	Website updates E-Alert and flyers to Bay Bridge contacts, including Treasure Island/YBI residents, taxis and shuttle services, airports, hotels, car rental agencies, visitor's bureaus, Chambers of Commerce, hospitals, major employers, entertainment venues, city and county governments, transit, and tourism agencies Transit Ridership Outreach MTC/511 Coordination Caltrans Employee Notification
August 2007	Public Service Announcements begin E-Alert to Elected Officials Reminder E-Alert to Bay Bridge Contacts Banners posted Electronic Message Signs and HAR begin Media Advisory
Labor Day Weekend 2007	Weekend site access for media PIO Live Updates Press Release announcing re-opening of Bay Bridge

Item 4: TBSRP Fact Sheet

Made In U.S.A.

Project*	Domestic Steel Content	Value of Domestic Steel
Bay Bridge East Span	76%	\$256 million
Bay Bridge West Span Retrofit	73%	\$42 million
New Benicia-Martinez Bridge	100%	\$81 million
Benicia-Martinez Bridge Retrofit	99%	\$23 million
Carquinez Bridge Replacement	45%	\$14 million
Carquinez Bridge Retrofit	53%	\$7 million
Richmond-San Rafael Bridge Retrofit	100%	\$133 million
San Mateo-Hayward Bridge Widening	100%	\$29 million
San Mateo-Hayward Bridge Retrofit	100%	\$18 million
Total	80%	\$603 million

*Does not include contracts yet to be awarded

Key Milestones Ahead

- Late 2007: Skyway section of new Bay Bridge East Span completed
- 2009: Traffic detours onto temporary structure between Yerba Buena Island tunnel and existing east span, allowing work crews to construct a tie-in between the tunnel and the new self-anchored suspension span
- 2009: Bay Bridge West Approach reconstruction completed
- 2012: New Bay Bridge East Span opens for west-bound traffic
- 2013: New Bay Bridge East Span opens for east-bound traffic.



Replacement of the mile-long Bay Bridge West Approach structure in San Francisco involves an intricate balance of construction and demolition.

More Bridge Closures, Extensive Public Outreach

As work on the Bay Bridge East Span replacement proceeds, occasional closures of the existing bridge (in one or both directions) will be necessary to ensure public safety while critical tasks are completed. In each case, the TBPOC will mount a full-scale public information blitz — and work with transit operators to provide additional transbay service — to keep traffic impacts to a minimum. The model was established in 2006, when the eastbound Bay Bridge was closed over two weekends (in June and again over the three-day Labor Day weekend) to allow work crews to demolish more than 1,000 feet of seismically fragile roadway.

Expect the Unexpected

Given the size, scope and schedule of the remaining Toll Bridge Seismic Retrofit Program projects, unforeseen engineering and construction challenges can be expected to arise in the years to come. The TBPOC will continue to look for ways to accelerate project delivery and reduce overall costs, and to mitigate environmental impacts associated with the projects.

For more information, visit:

www.baybridgeinfo.org
bata.mtc.ca.gov



TOLL BRIDGE PROGRAM OVERSIGHT COMMITTEE

CALTRANS BAY AREA TOLL AUTHORITY CALIFORNIA TRANSPORTATION COMMISSION

TOLL BRIDGE SEISMIC RETROFIT PROGRAM

7 of 9 Projects NowComplete

Bay Area Toll Bridge Seismic Retrofit Projects	Seismic Safety Status
San Francisco-Oakland Bay Bridge East Span Replacement	Construction
San Francisco-Oakland Bay Bridge West Approach Replacement	Construction
San Francisco-Oakland Bay Bridge West Span Seismic Retrofit	Completed
San Mateo-Hayward Bridge Seismic Retrofit	Completed
Richmond-San Rafael Bridge Seismic Retrofit	Completed
Eastbound Carquinez Bridge Seismic Retrofit	Completed
Benicia-Martinez Bridge Seismic Retrofit	Completed
San Diego-Coronado Bridge Seismic Retrofit	Completed
Vincent Thomas Bridge Seismic Retrofit	Completed

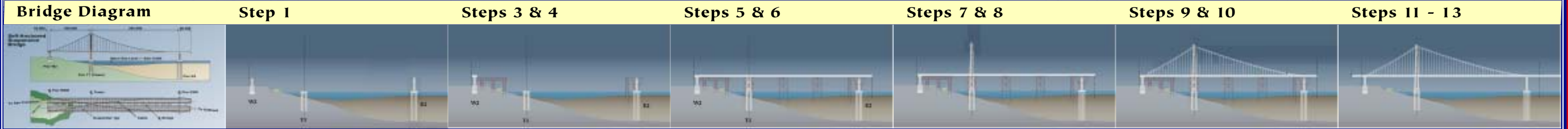
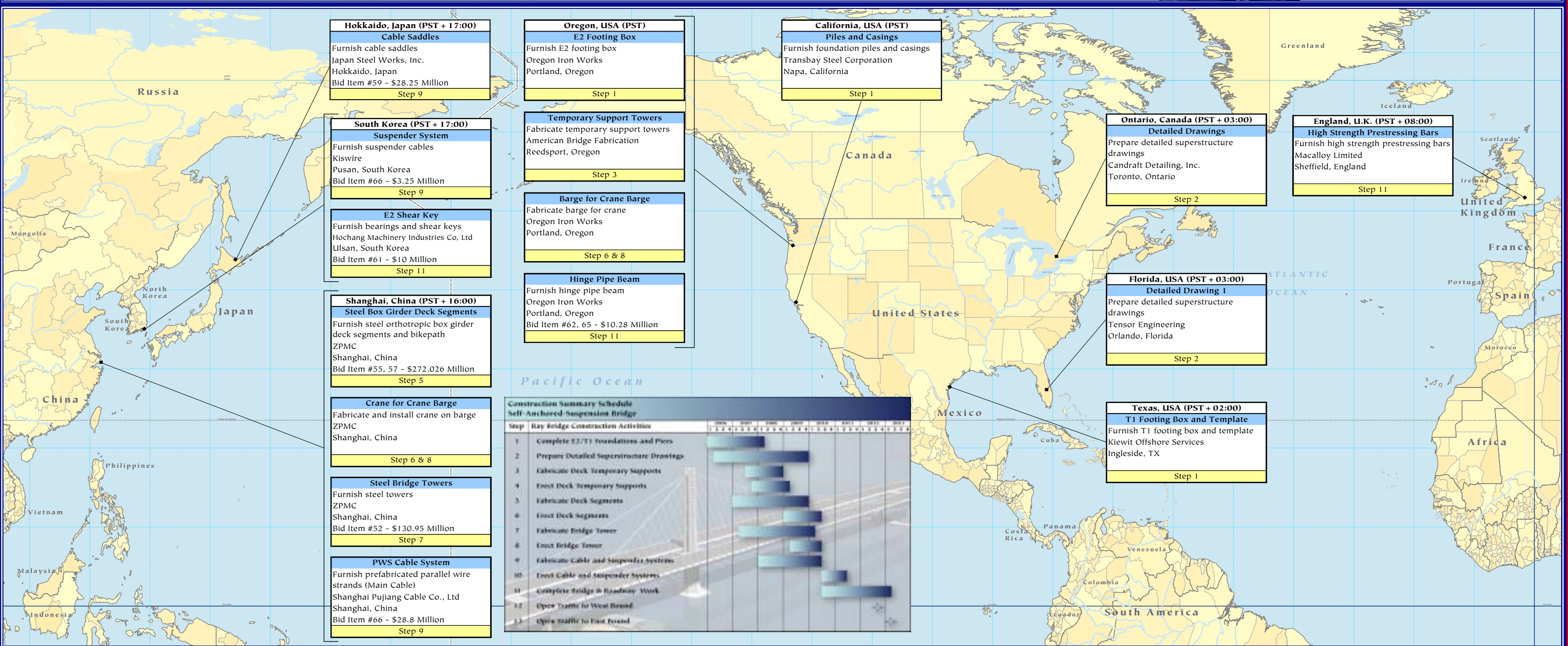
Bay Bridge Construction Update

The new Bay Bridge East Span is a project with a truly global scope. In addition to the fabrication and assembly work being done in California, portions of the nearly \$5.5 billion structure are being sourced from Florida, Oregon and Texas, as well as from Canada, China, England, Japan and South Korea.

(Continued on back page)



The final segment of the skyway portion of the new Bay Bridge East Span is lifted into place in December, 2006.



**Item 5: Verbal Update on Cameras Linked to
Website
(No Attachment)**